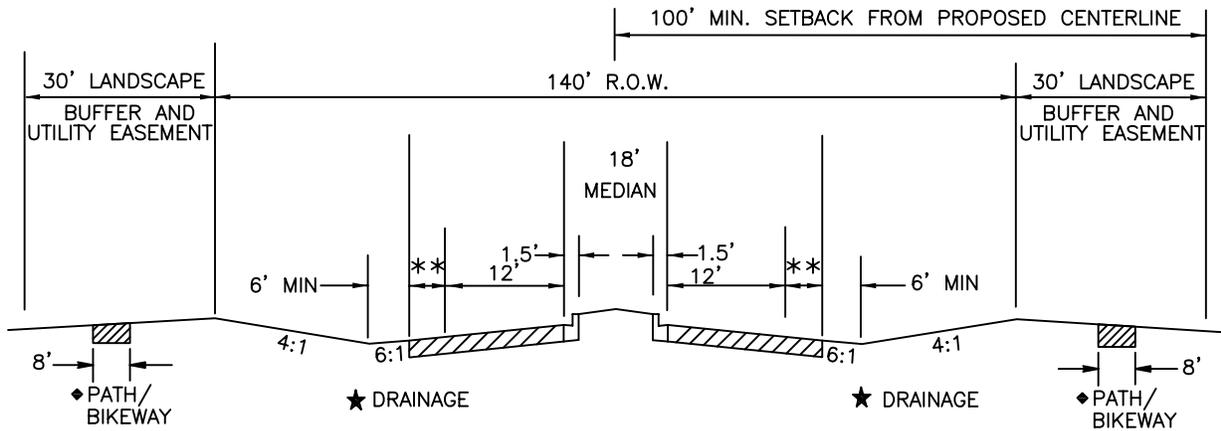


INDEX OF DRAWINGS

STREETS

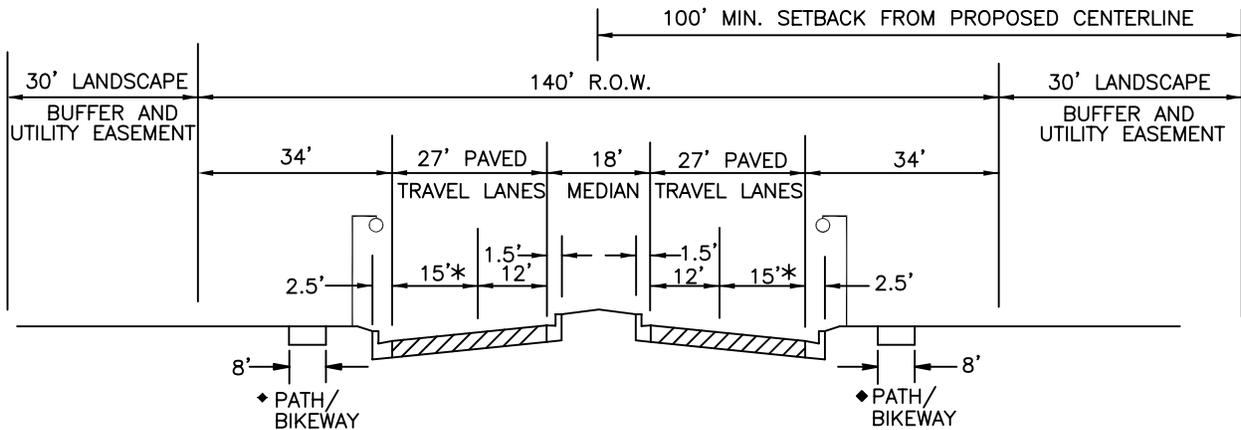
<u>DRAWING NO.</u>	<u>TITLE</u>
ST1A	PRINCIPAL ARTERIAL
ST1B	PRINCIPAL ARTERIAL
ST2	MINOR ARTERIAL
ST3	COLLECTORS
ST4	COLLECTORS
ST5	SIGHT DISTANCE
ST6	LOCAL STREETS
ST7	PAVEMENT PHASING—NEW ROADS
ST8	RURAL ARTERIAL
ST9	DRIVEWAY APPROACHES FOR ROADS
ST10	TRENCH & CURB PATCH
ST11	STRUCTURE PATCH
ST12	PORTLAND CEMENT REPLACEMENT
ST13	90° TURN — LOCAL ACCESS STREETS
ST14	CUL—DE—SACS
ST15A	GROUND MOUNT STREET NAME SIGN
ST15B	ROAD AND STREET NAME SIGNS
ST15C	PRIVATE STREET SIGN
ST16	4" PERFORATED UNDERDRAIN
ST17A	4" PERFORATED MEDIAN UNDERDRAIN FOR CENTER PLANTING
ST17B	4" PERFORATED MEDIAN UNDERDRAIN FOR EDGE PLANTING
ST18	UNDERDRAIN OUTLET TREATMENT
ST19	TYPICAL STREET UTILITY LOCATION



INITIAL PHASE

** 6' SHOULDER TO ACCOMMODATE BICYCLES

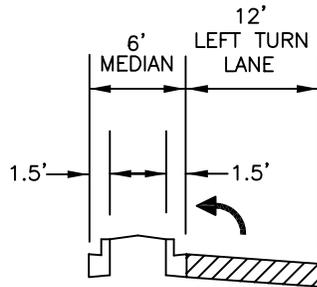
★ DRAINAGE DITCH TO BE ENGINEERED AT CROSS STREETS AND DRIVEWAYS



INTERMEDIATE PHASE

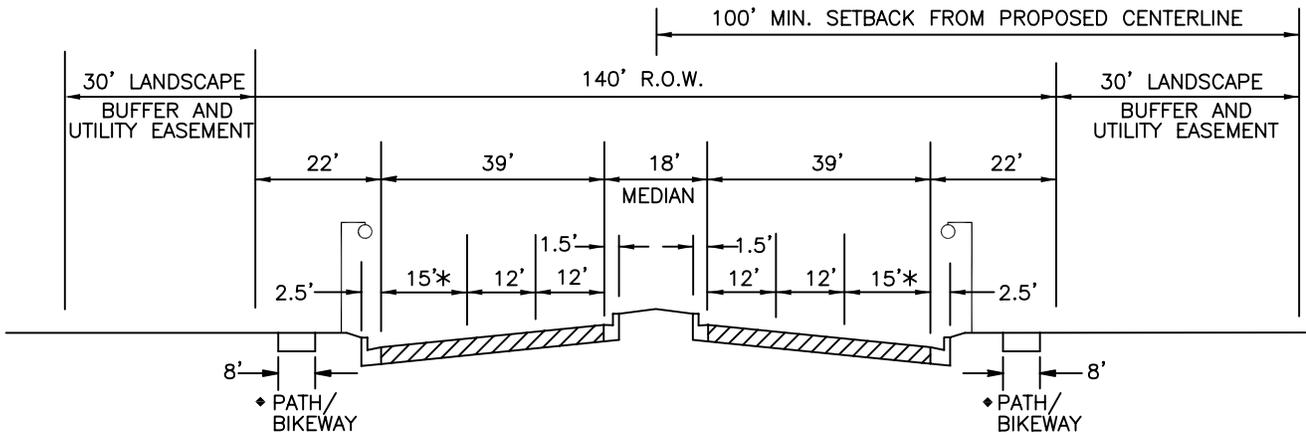
◆ PATH MAY BE LOCATED IN LANDSCAPE BUFFER. PATH SHOULD MEANDER—SEE PARKS AND RECREATION STANDARDS





SEE LANDSCAPE PLANS
FOR MEDIAN TREATMENT

TYPICAL MEDIAN W/LEFT TURN LANE

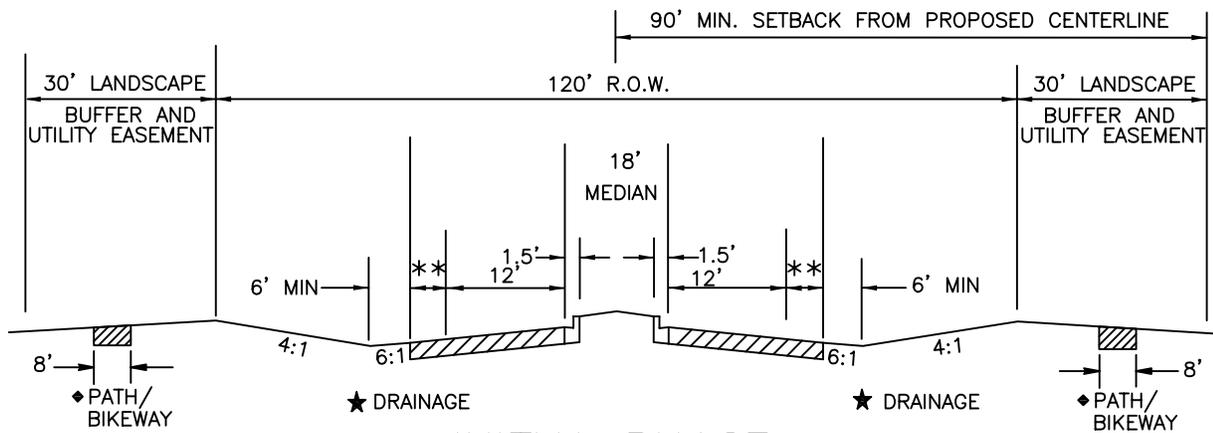


ULTIMATE ROADWAY

*OUTSIDE LANE IS 15' WIDE TO ACCOMADATE BICYCLES

♦ PATH MAY BE LOCATED IN LANDSCAPE BUFFER. PATH SHOULD MEANDER—SEE PARKS AND RECREATION STANDARDS



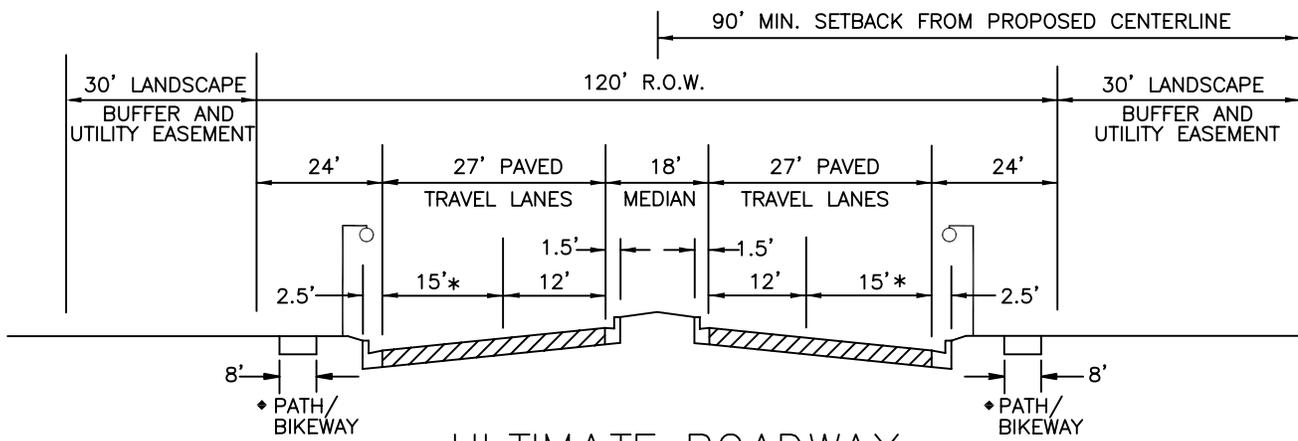


INITIAL PHASE

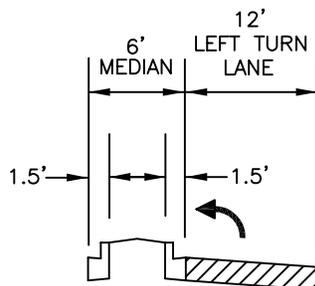
◆ PATH MAY BE LOCATED IN LANDSCAPE BUFFER. PATH SHOULD MEANDER—SEE PARKS AND RECREATION STANDARDS

** 6' SHOULDER TO ACCOMMODATE BICYCLES

★ DRAINAGE DITCH TO BE ENGINEERED AT CROSS STREETS AND DRIVEWAYS

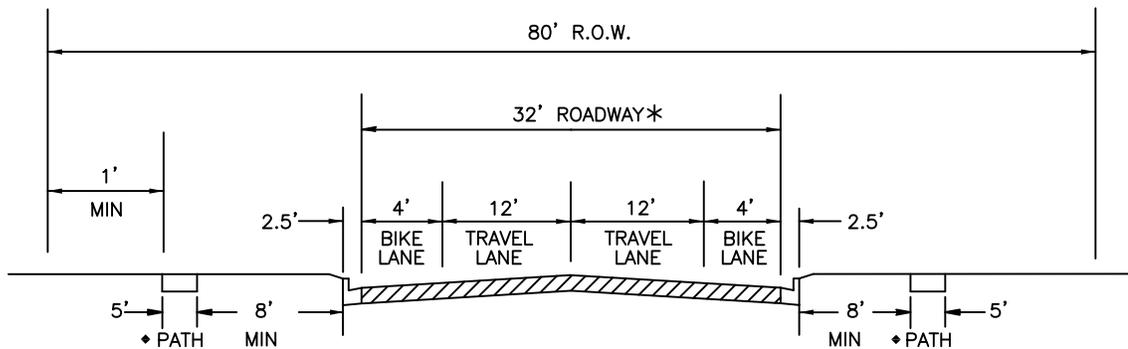


ULTIMATE ROADWAY



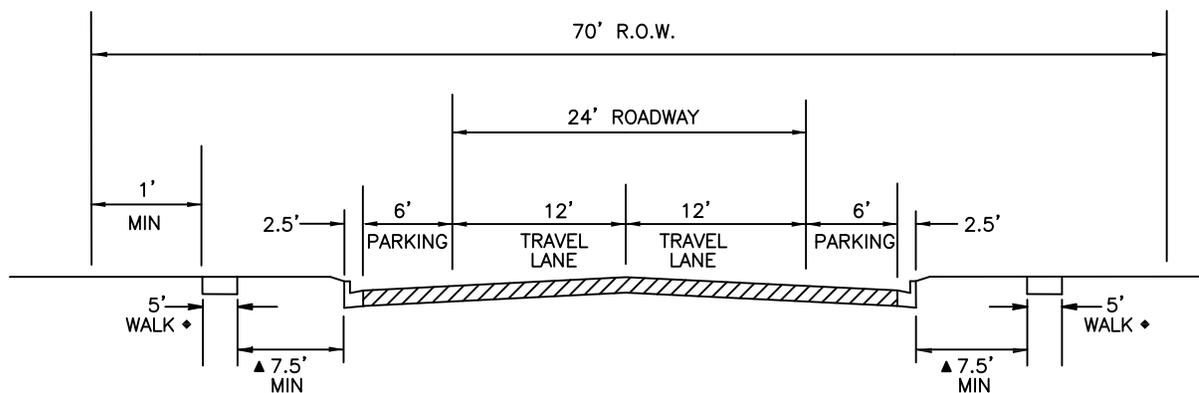
TYPICAL MEDIAN W/LEFT TURN LANE





COLLECTOR WITHOUT PARKING OR MEDIAN

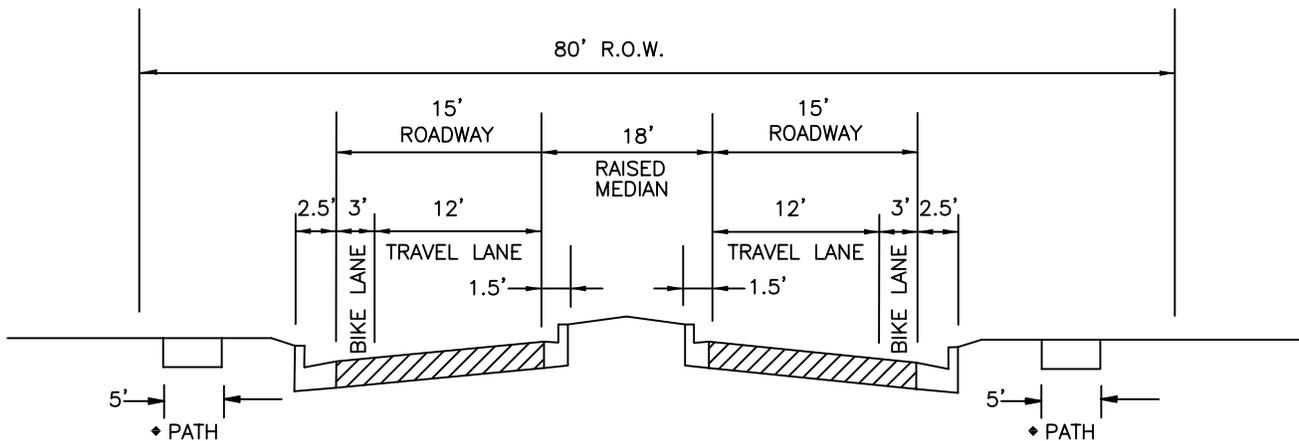
- NOTE:
 PRIVATE UTILITIES TO BE PLACED OUTSIDE OF THE PUBLIC RIGHT-OF-WAY
 * TURN LANES WILL BE REQUIRED AS DETERMINED BY A TRAFFIC STUDY
 ♦ PATH SHOULD MEANDER—SEE PARKS AND RECREATION STANDARDS



RESIDENTIAL COLLECTOR WITH ON-STREET PARKING
 (TYPICAL VOLUME: UP TO 2,000 VPD)

▲ 8' MINIMUM IN MULTI-FAMILY DEVELOPMENT

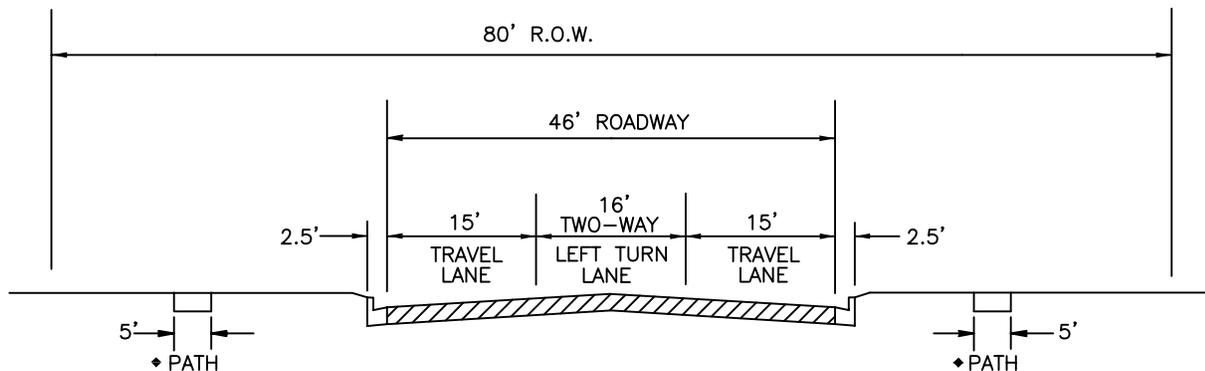




COLLECTOR WITH RAISED MEDIAN

NOTE:

- PRIVATE UTILITIES TO BE PLACED OUTSIDE OF THE PUBLIC RIGHT-OF-WAY
- ◆ PATH SHOULD MEANDER—SEE PARKS AND RECREATION STANDARDS



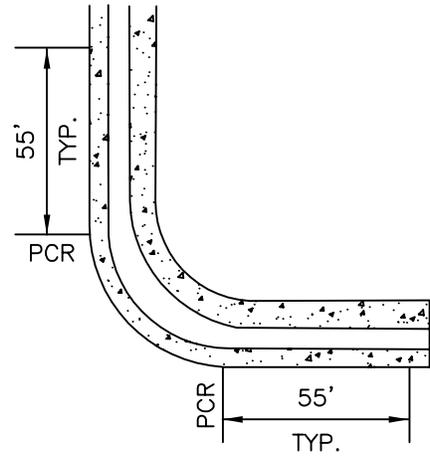
COLLECTOR WITH FLUSH MEDIAN
(PRIMARYLY INTENDED FOR INDUSTRIAL AREAS)

NOTE:

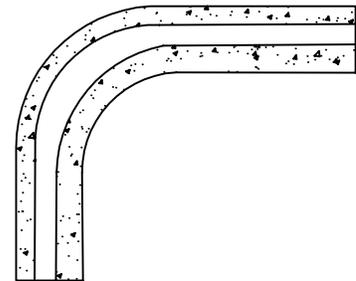
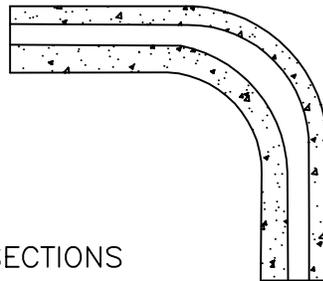
- PRIVATE UTILITIES TO BE PLACED OUTSIDE OF THE PUBLIC RIGHT-OF-WAY
- ◆ PATH SHOULD MEANDER—SEE PARKS AND RECREATION STANDARDS



DETACHED WALK
TREE LAWN (TYP)



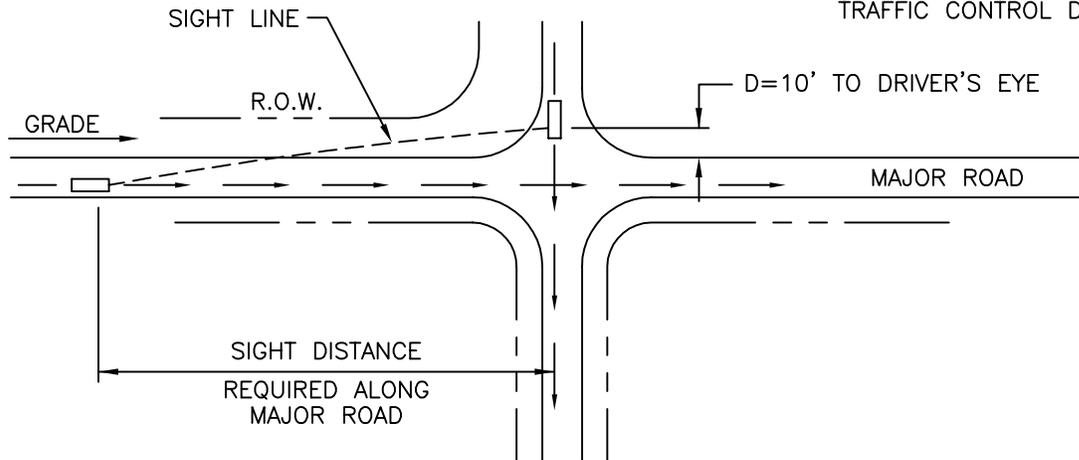
NO TREES IN TREE LAWN WITHIN 55 FEET OF PCR. ANY OTHER PLANTS AND LANDSCAPING MUST BE APPROVED BY THE PARKS AND RECREATION DIRECTOR OR DESIGNEE.



LOCAL STREET INTERSECTIONS

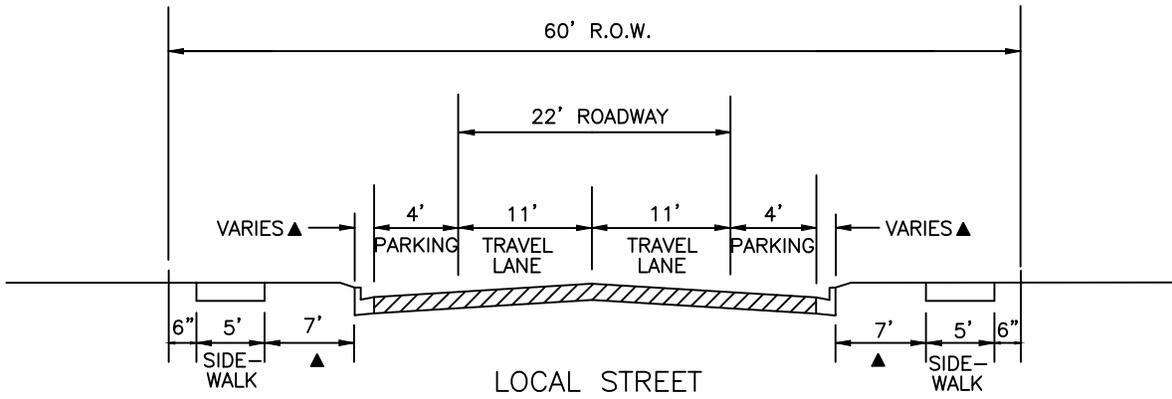
STOPPED APPROACH

SIGHT DISTANCE REQUIREMENT FOR INTERSECTIONS WITH TRAFFIC CONTROL DEVICES



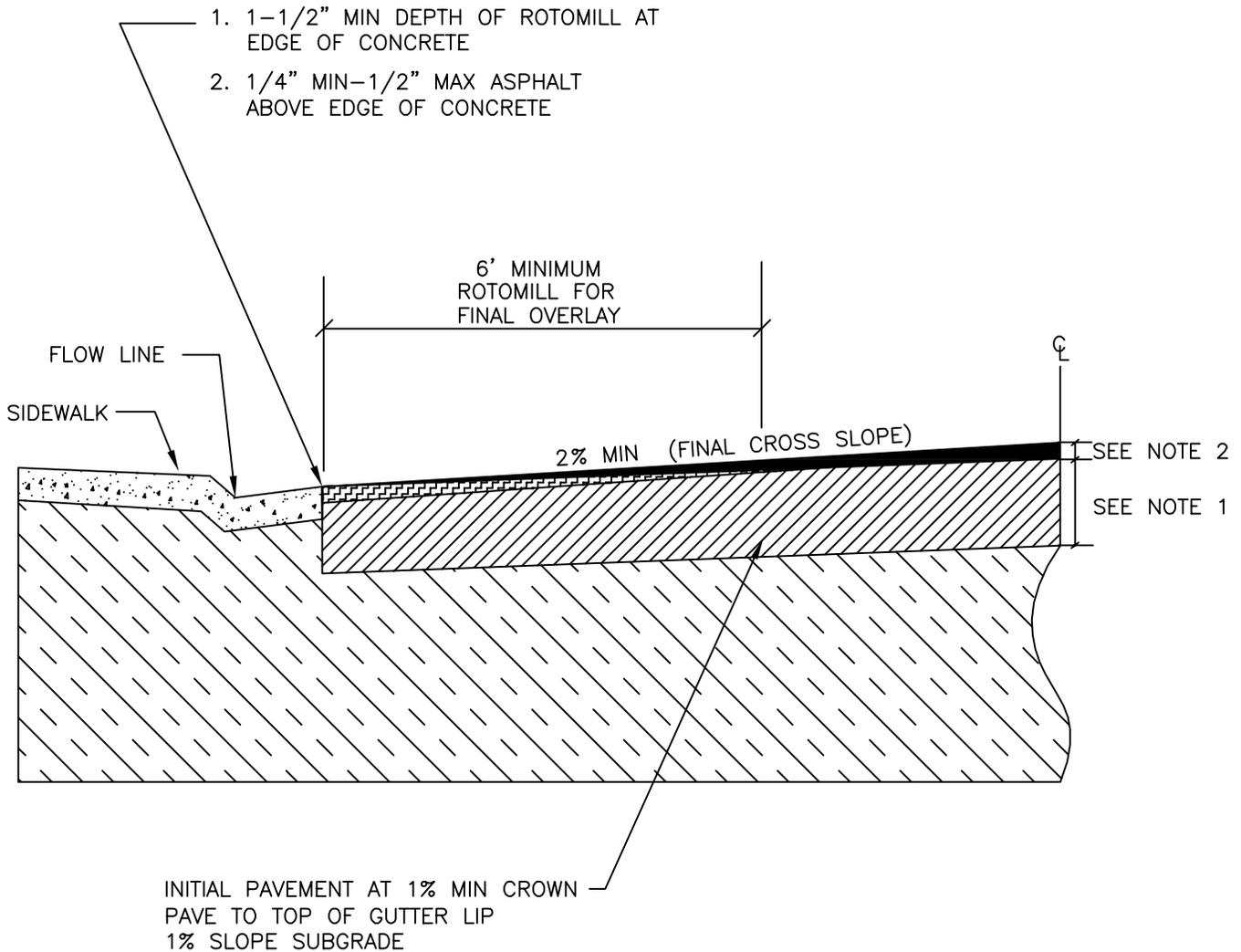
DESIGN SPEED OF THRU ROADWAY (MPH)	MINIMUM SIGHT DISTANCE FOR STOPPED VEHICLE (FEET)	GRADE CORRECTION DISTANCE (FEET)				
		SPEED	UPGRADE TO	FOR DOWNGRADES		
			3%	6%	3%	6%
25	250	25	0	-10	+10	+20
30	300	30	0	-10	+10	+20
35	350	35	-10	-15	+10	+25
40	400	40	-10	-20	+10	+30
45	450	45	-15	-25	+15	+40





NOTE:
 PRIVATE UTILITIES TO BE OUTSIDE OF THE PUBLIC RIGHT-OF-WAY
 ▲ WIDTH DEPENDS ON VERTICAL OR ROLLOVER CURB

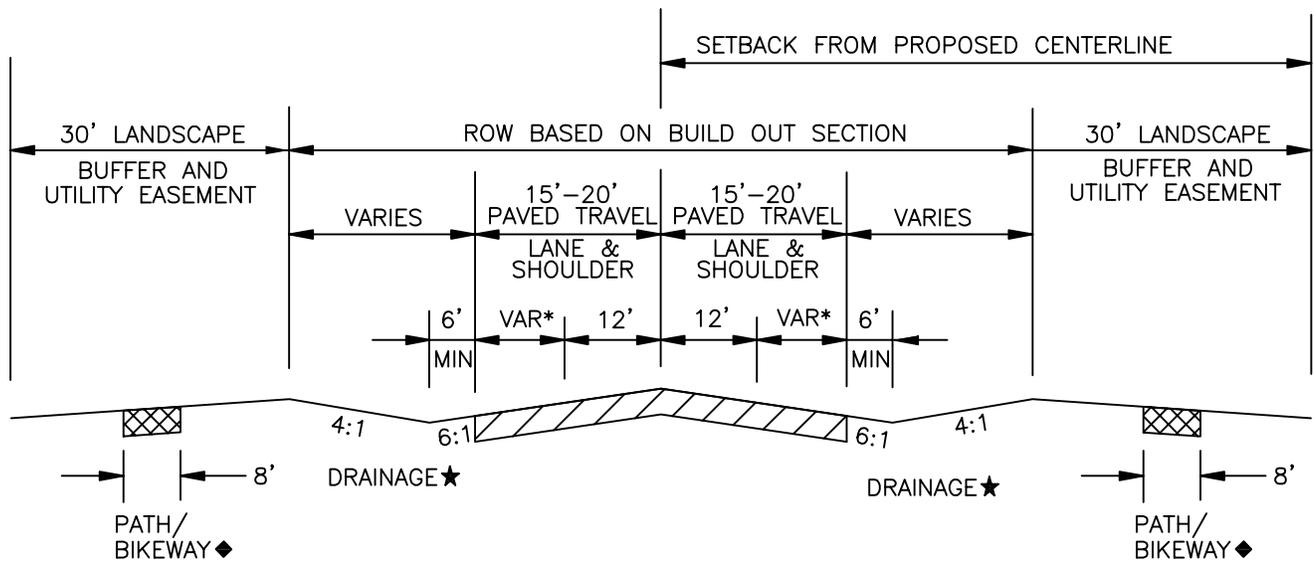




NOTES:

1. INITIAL PAVEMENT DEPTH AT CONSTRUCTION ACCEPTANCE SHALL BE THE FULL DESIGN DEPTH FOR THE ENTIRE WIDTH OF THE ROAD SECTION.
2. FINAL OVERLAY DEPTH AT FINAL ACCEPTANCE IS IN ADDITION TO THE DESIGN DEPTH. THE DEPTH WILL VARY DEPENDING UPON THE PAVEMENT WIDTH TO ENSURE A MINIMUM OF A 2% PAVEMENT CROSS SLOPE.
3. THE FINAL OVERLAY SHALL BE COMPLETE AT FINAL ACCEPTANCE WHICH IS AFTER THE END OF THE 2 YEAR WARRANTY PERIOD WHEN ALL FINAL PUNCH LIST REPAIRS ARE COMPLETE.
4. DETERMINATION OF CROWN FOR CUL DE SAC PAVING SHALL BE EVALUATED ON A CASE BY CASE BASIS.





RURAL ARTERIAL

*BIKES CAN USE PAVED SHOULDERS

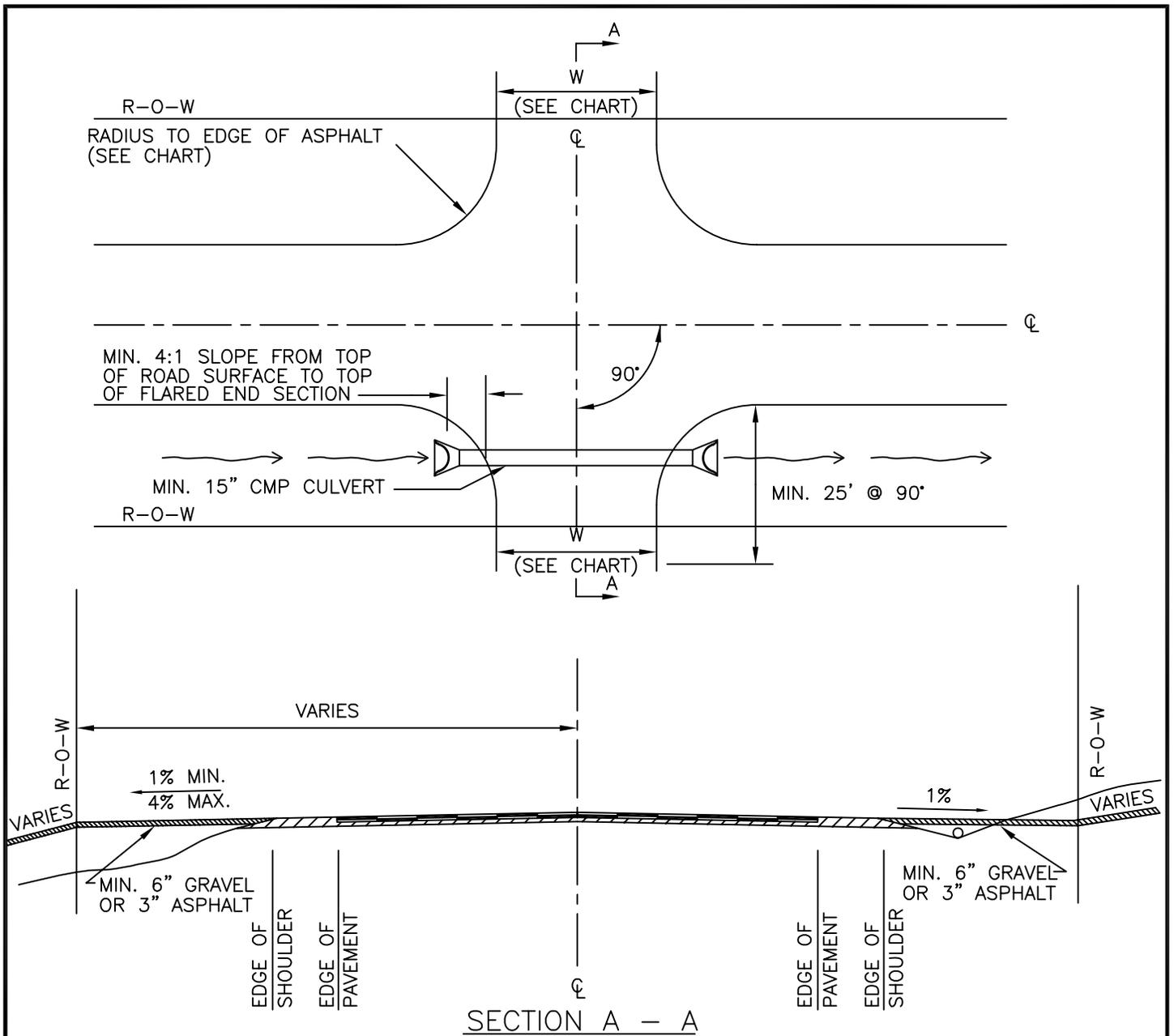
SHOULDER WIDTH VARIES DEPENDING ON DESIGN VOLUME:

- 4' WIDTH - UP TO 5,000 VPD
- 6' WIDTH - 5,000 TO 10,000 VPD
- 8' WIDTH - OVER 10,000 VPD

◆PATH (5' TO 10') AT THE DISCRETION OF THE TOWN OF ERIE. PATH SHOULD MEANDER - SEE PARKS AND RECREATION STANDARDS

★DRAINAGE DITCH TO BE ENGINEERED AT CROSS STREETS AND DRIVEWAYS





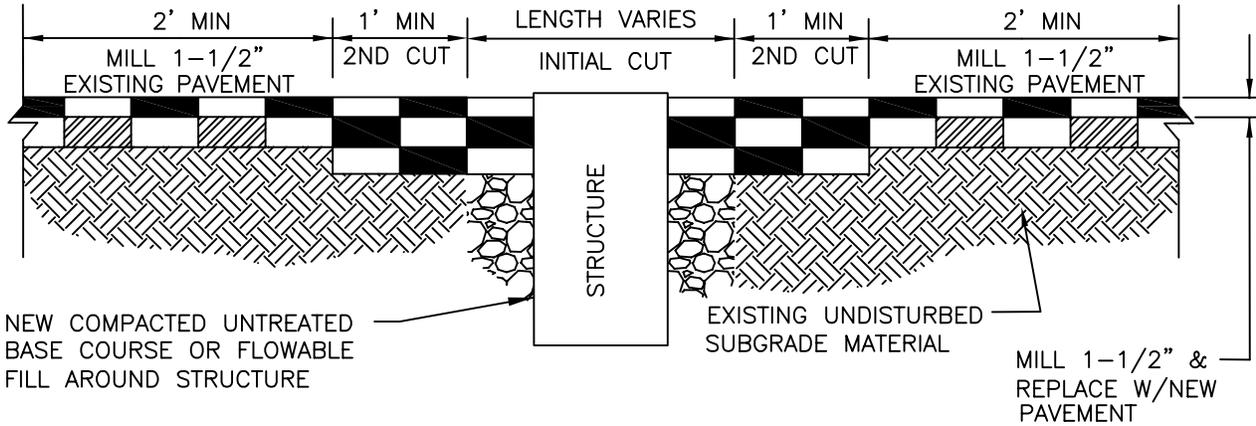
SECTION A - A

	DRIVEWAY WIDTH (FEET)	PAVEMENT RADIUS (FEET)
RESIDENTIAL	10-25	10
NON-RESIDENTIAL	15-35	15

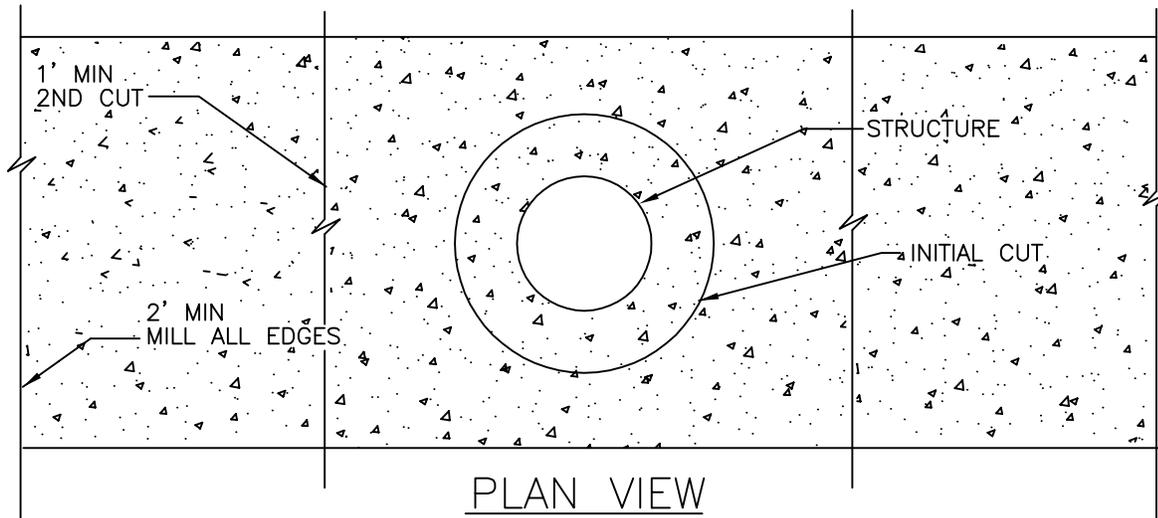
NOTES

1. THE TOWN OF ERIE ENGINEERING DEPARTMENT WILL DETERMINE THE REQUIRED CULVERT SIZE. FLARED END SECTIONS ARE REQUIRED FOR CULVERTS 24" IN DIAMETER OR LARGER
2. DRIVEWAY PERMITS ARE REQUIRED FROM THE TOWN OF ERIE ENGINEERING DEPARTMENT
3. THE TOWN OF ERIE ENGINEERING DEPARTMENT MUST REVIEW DRIVEWAY LOCATIONS ONTO COLLECTOR AND ARTERIAL ROADS PRIOR TO A PERMIT BEING ISSUED.





PATCHBACK FOR STRUCTURES (MANHOLES, VALVES ETC)



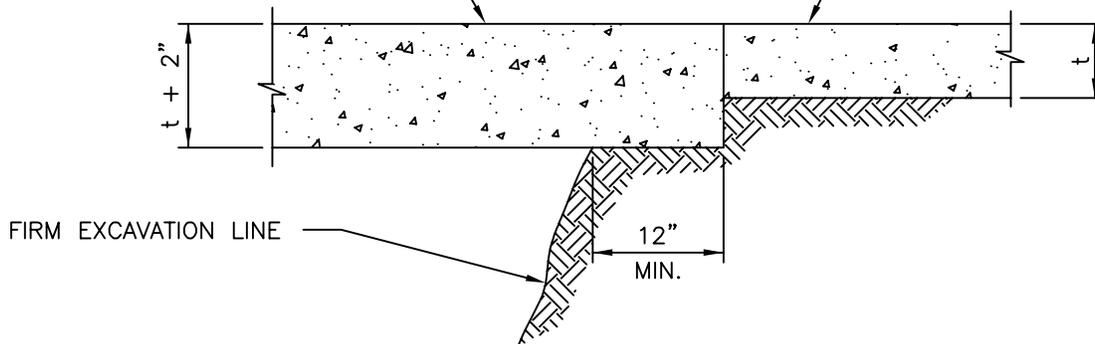
NOTE:

1. IF ASPHALT PATCH THICKNESS IS NOT IDENTIFIED ON PLANS USE 6-1/2" MIN ASPHALT PATCH OR MATCH EXISTING, WHICH EVER THICKNESS IS GREATER.
2. MINIMUM DEPTH OF WEARING COURSE SHALL BE 1-1/2" AND SHALL BE GRADING SX ASPHALT.
3. MINIMUM DEPTH OF INTERMEDIATE COURSE SHALL BE 5" AND BE INSTALLED IN 2 LIFTS. INTERMEDIATE COURSE SHALL BE GRADING S OR G ASPHALT.
4. PATCH SHALL BE PLACED AND COMPACTED IN LIFTS A MAXIMUM OF 3" IN DEPTH.
5. APPLY SS-I TACK COAT TO EXISTING ASPHALT AND/OR CONCRETE VERTICAL SURFACES.
6. TRENCHES LESS THAN 2' IN WIDTH MUST RECEIVE PRIOR APPROVAL FROM THE TOWN OF ERIE ENGINEERING DEPARTMENT AND SHALL BE FLOW-FILLED.
7. PROVIDE 28 DAY 60 PSI CONTROLLED LOW STRENGTH FLOWABLE FILL AS SPECIFIED. USE FILL THAT FLOWS EASILY AND VIBRATION IS NOT REQUIRED. CURE TO INITIAL SET BEFORE PLACING NEW UNTREATED BASE COURSE OR NEW ASPHALT PAVEMENT. USE FLOWABLE FILL IN EXCAVATIONS THAT ARE TOO NARROW TO RECEIVE COMPACTION EQUIPMENT.
8. REMOVE ADDITIONAL PAVEMENT TO A PAINTED LANE STRIPE, A LIP OF GUTTER, A CURB, AN EXISTING PAVEMENT PATCH, OR AN EDGE OF THE PAVEMENT IF SUCH STREET FEATURE IS WITHIN TWO FEET OF THE SECOND SAW CUT.
9. PROVIDE UNTREATED BASE COURSE MATERIAL. DO NOT USE GRAVEL OR WASHED ROCK. PLACE NEW MATERIAL IN LIFTS NOT EXCEEDING 8" AFTER COMPACTION. COMPACT TO A MODIFIED PROCTOR DENSITY OF 95% OR GREATER.
10. STRAIGHT SAWCUT OR BLADECUT THE EXISTING ASPHALT PAVEMENT WHEN JOINING WITH NEW ASPHALT PAVEMENT.



NEW P.C. CONCRETE PAVEMENT

EXISTING P.C. CONCRETE PAVEMENT

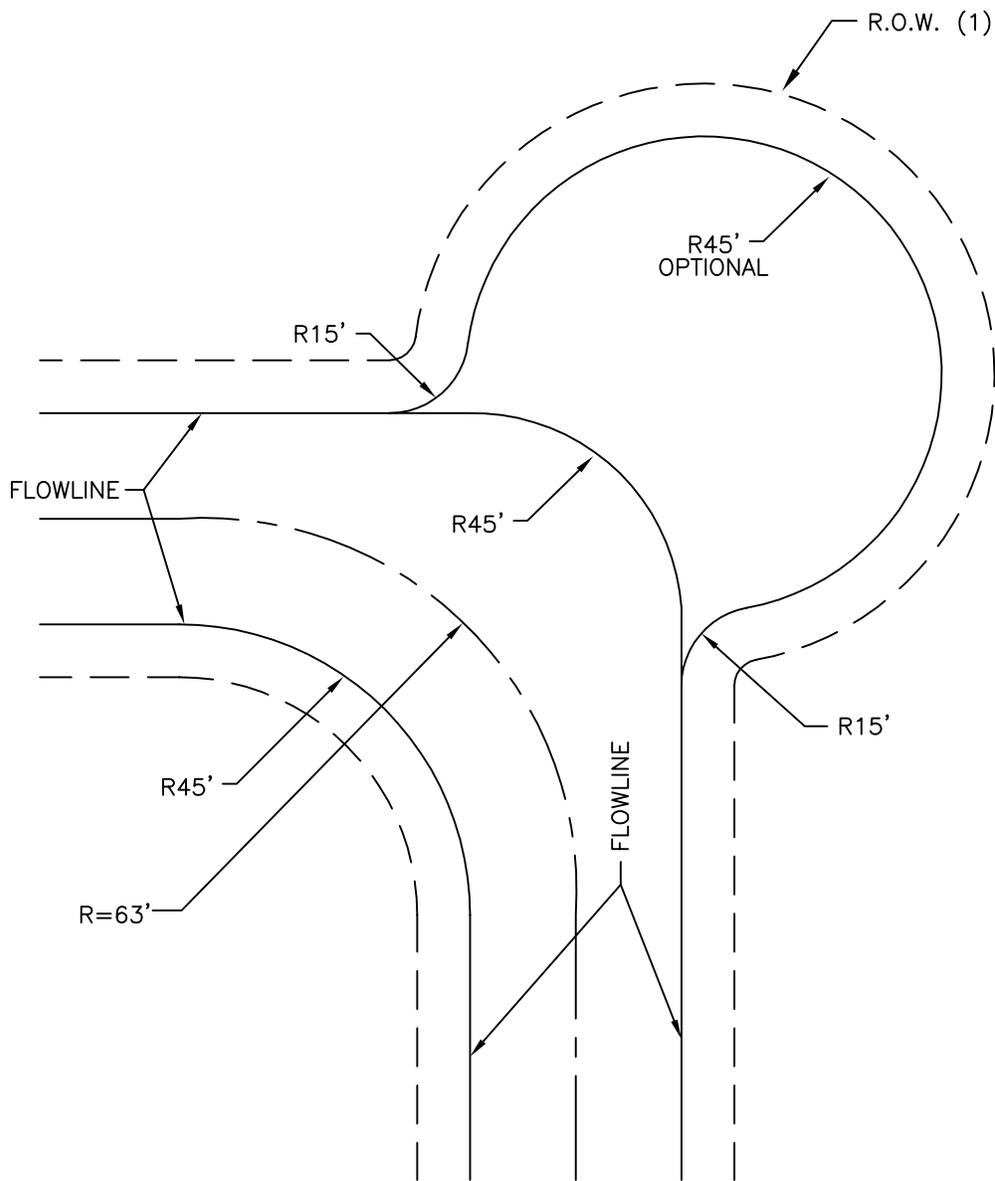


NOTE:

WHERE EXISTING PAVEMENT CONTAINS REINFORCING, THE REPLACEMENT SHALL BE REINFORCED AND DOWELED AS DIRECTED BY THE ENGINEER.

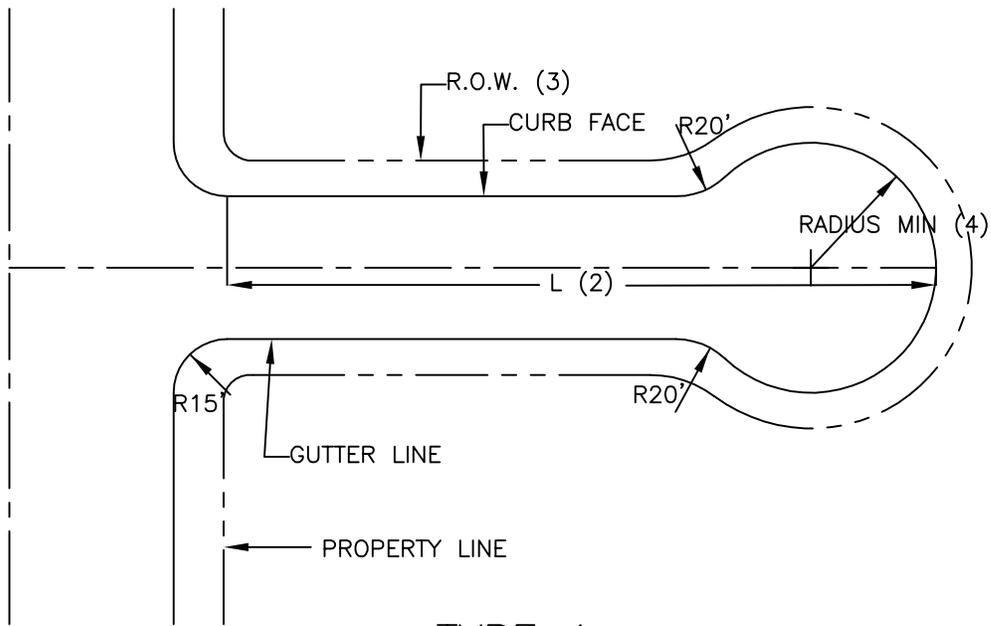
PORTLAND CEMENT CONCRETE SURFACE



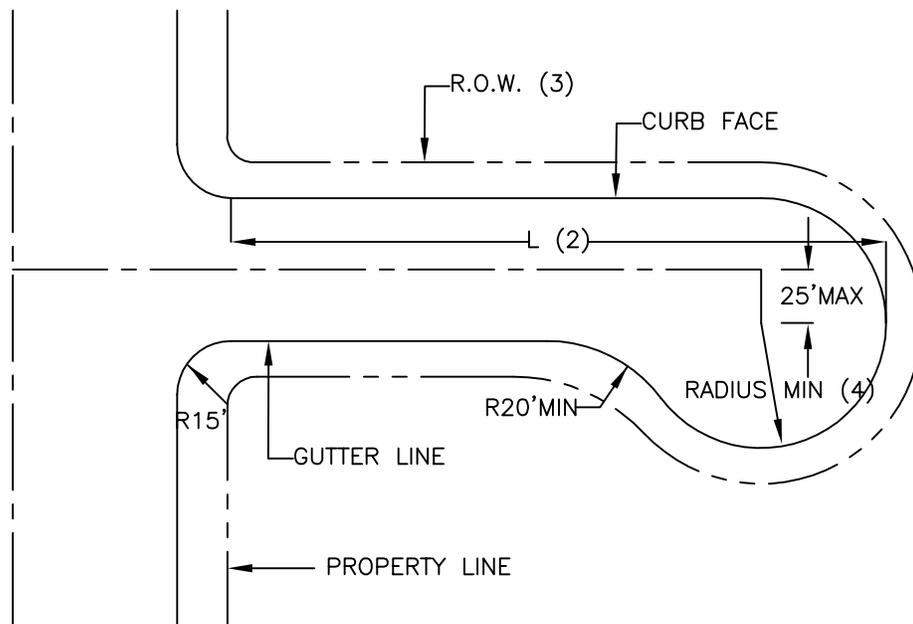


1. 20MPH ADVISORY SIGNS REQUIRED ON CURVE APPROACHES





TYPE 1



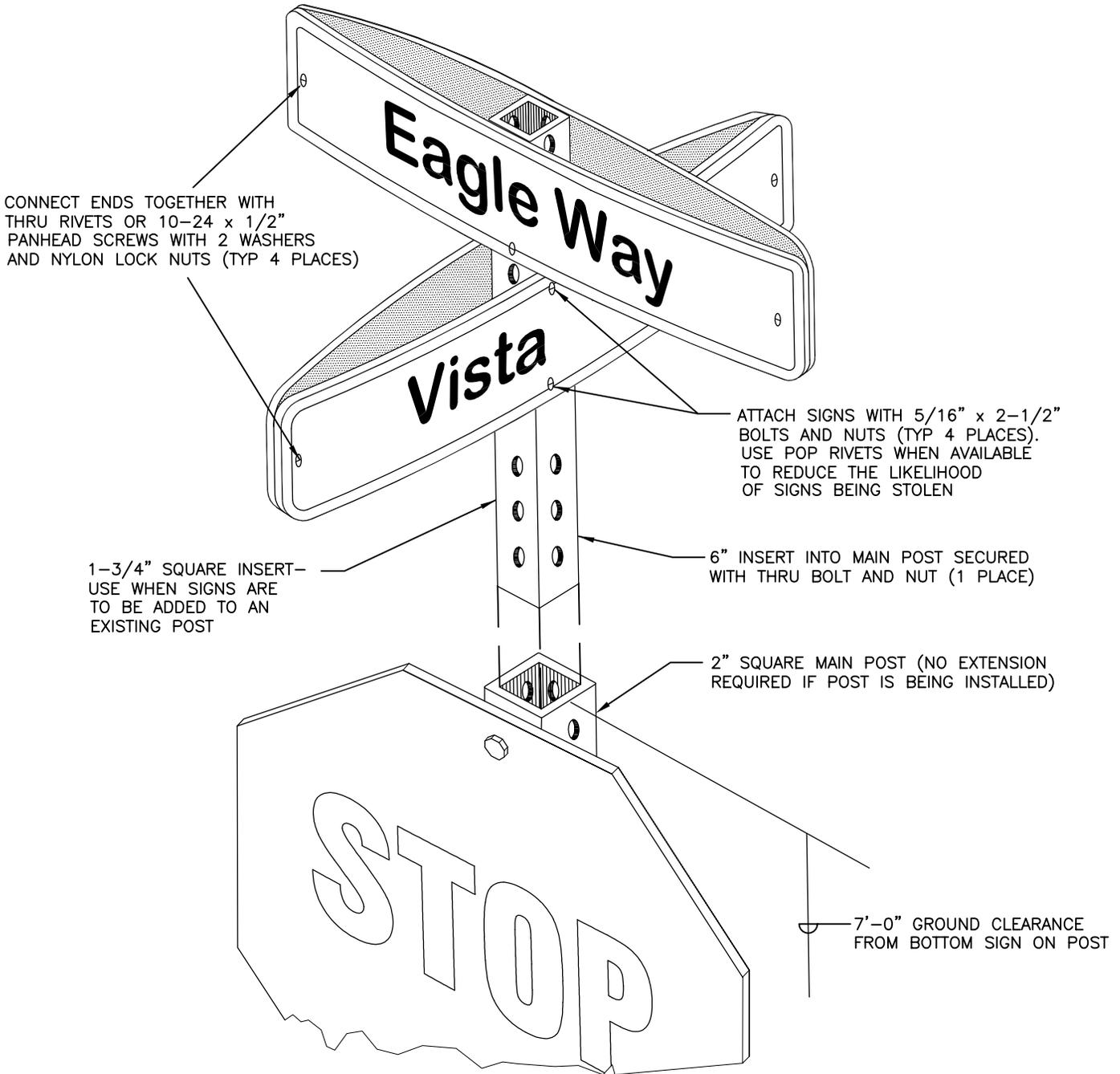
TYPE 2

NOTES:

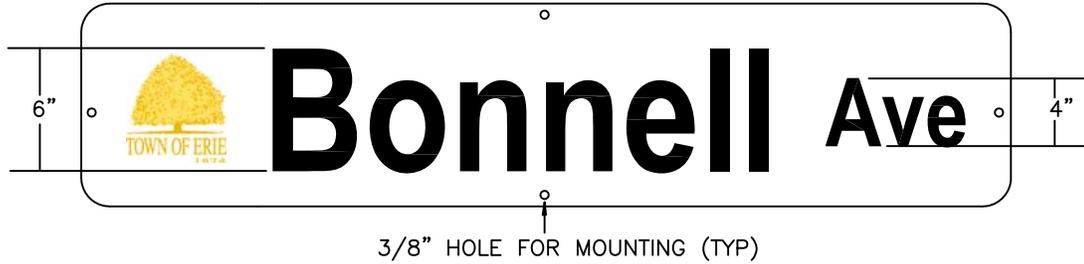
- (1) WIDTH OF TYPE 1 & 2 CUL-DE-SACS TO CONFORM TO APPLICABLE TYPICAL STREET CROSS SECTIONS.
- (2) SEE SECTION 526.00 CUL-DE-SACS FOR LENGTH (L) REQUIREMENTS.
- (3) R.O.W. LINES ARE TO BE PARALLEL TO AND OFFSET FROM THE CURB FACE AND THE DISTANCE SHALL MATCH THE STREET SECTION DETAIL.
- (4) R = 35' IN RESIDENTIAL AREAS AND R = 45' IN COMMERCIAL/INDUSTRIAL AREAS.



STANDARD ORIENTATION:
NORTH-SOUTH STREETS ON TOP
EAST-WEST STREETS ON BOTTOM



9" STREET SIGN WITH TOWN OF ERIE LOGO



6" STREET SIGN WITHOUT TOWN OF ERIE LOGO

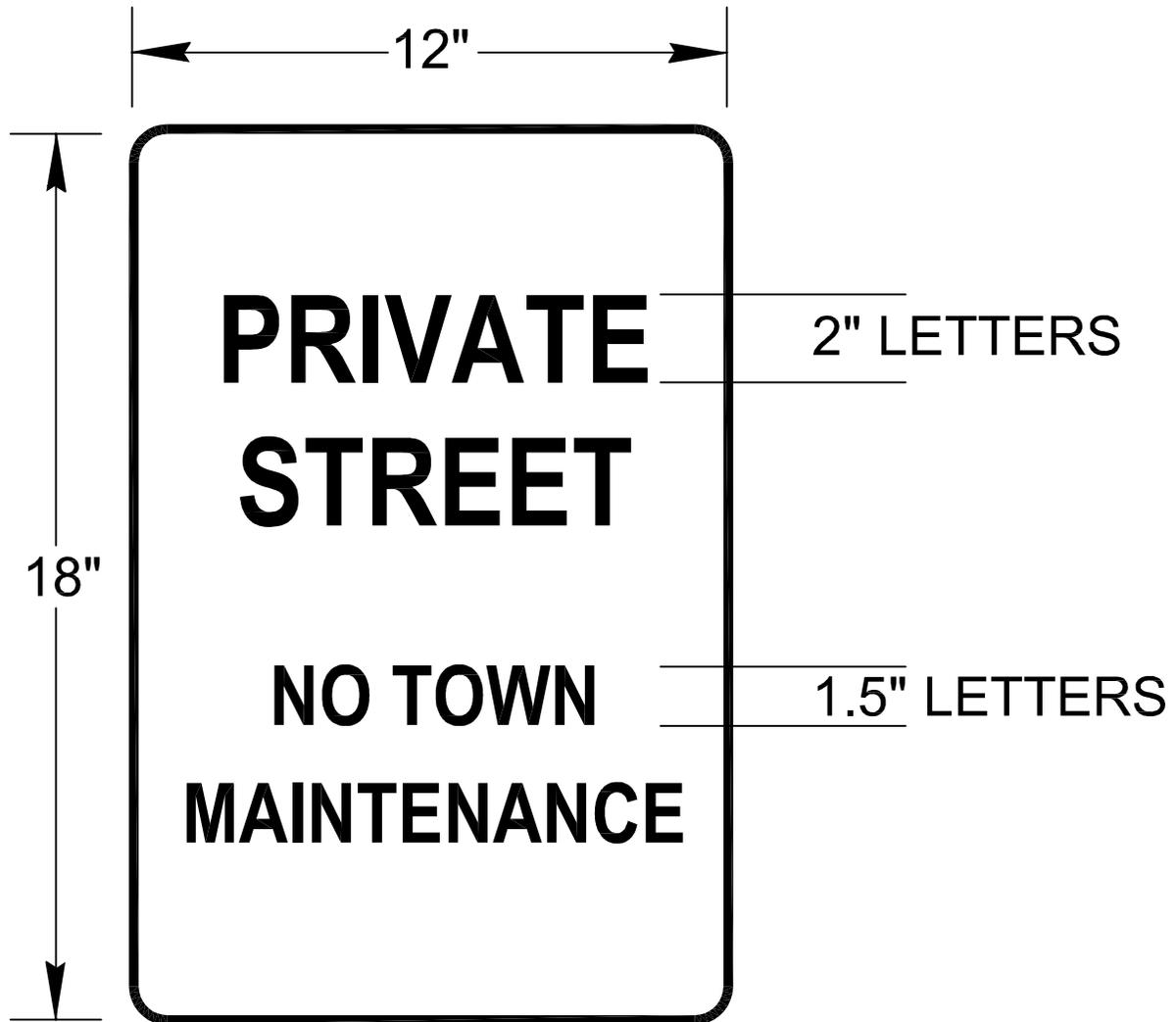


INTERSECTION TYPE	SIGN BLANK SIZE (30" MINIMUM ON ALL LENGTHS)	MOUNTING	RECOMMENDED MINIMUM			
			LETTER HEIGHT		SUFFIX (ST, AVE, CT ETC)	
			INITIAL UPPER CASE	LOWER CASE	INITIAL UPPER CASE	LOWER CASE
ARTERIALS AT SIGNAL LIGHTS WITH LOGO		OVERHEAD	12 INCHES	9 INCHES	6 INCHES	4.5 INCHES
MULTI LANE ARTERIALS AND ALL OTHERS DIRECTED BY PUBLIC WORKS DEPT WITH LOGO	10" X AS NEEDED	POST MOUNTED	8 INCHES	6 INCHES	4.5 INCHES	3.5 INCHES
ARTERIALS & COLLECTORS AND ALL OTHERS DIRECTED BY PUBLIC WORKS DEPT WITH LOGO	9" X AS NEEDED	POST MOUNTED	6 INCHES	4.5 INCHES	4 INCHES	3 INCHES
LOCAL/NO LOGO	9" X AS NEEDED	POST MOUNTED	6 INCHES	4.5 INCHES	4 INCHES	3 INCHES
*LOCAL/NO LOGO	6" X AS NEEDED	POST MOUNTED	4 INCHES	3 INCHES	3 INCHES	2.25 INCHES
*ON LOCAL TWO-LANE STREETS WITH SPEED LIMITS OF 25 MPH OR LESS, 4 INCH INITIAL UPPER-CASE LETTERS WITH 3 INCH LOWER CASE LETTERS MAY BE USED						
NO BORDERS EXCEPT ON OVERHEAD LIGHTED STREET SIGNS AT SIGNALIZED INTERSECTIONS						
WHEN LOGO IS DISPLAYED, THE HEIGHT AND WIDTH OF THE LOGO SHALL NOT EXCEED THE UPPER CASE LETTER HEIGHT OF THE PRINCIPAL LEGEND OF THE SIGN AND LOCATED LEFT OF THE STREET NAME						

NOTES:

1. SIGN BLANKS SHALL BE 6061 OR 5052-H38 ALUMINUM ALLOY MIN .080" THICK.
2. FACING SHALL BE GREEN HI-INTENSITY RETROREFLECTIVE SHEETING.
3. LETTERS AND NUMBERS SHALL BE WHITE RETROREFLECTIVE SHEETING.
4. TOWN OF ERIE COLOR LOGO IS TO BE USED FOR COLLECTOR AND ARTERIAL STREET BLADES ONLY.
5. 30" MIN. LENGTH ON ALL STREET SIGNS, SINGLE FACE FOR BACK TO BACK INSTALLATION AND 1/4" HOLES FOR PINNING ENDS OF SIGNS TOGETHER

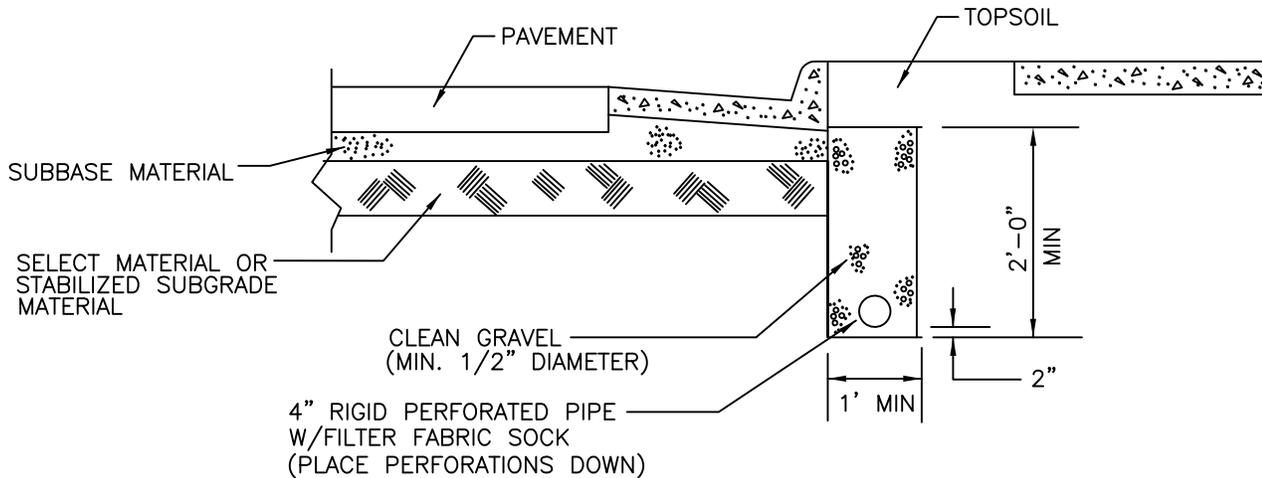




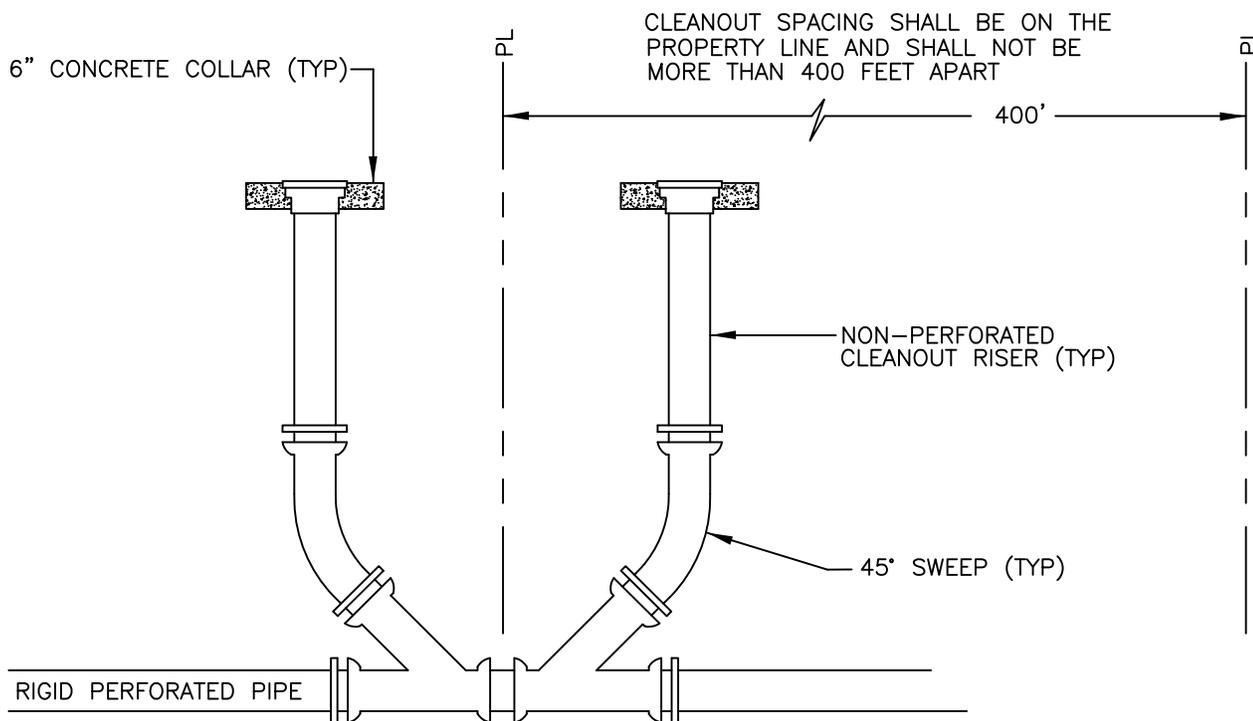
NOTES:

1. SIGN BLANKS SHALL BE 6061 OR 5052-H38 ALUMINUM ALLOY MIN .080" THICK.
2. FACING SHALL BE WHITE RETRO REFLECTIVE.
3. LETTERS SHALL BE BLACK.





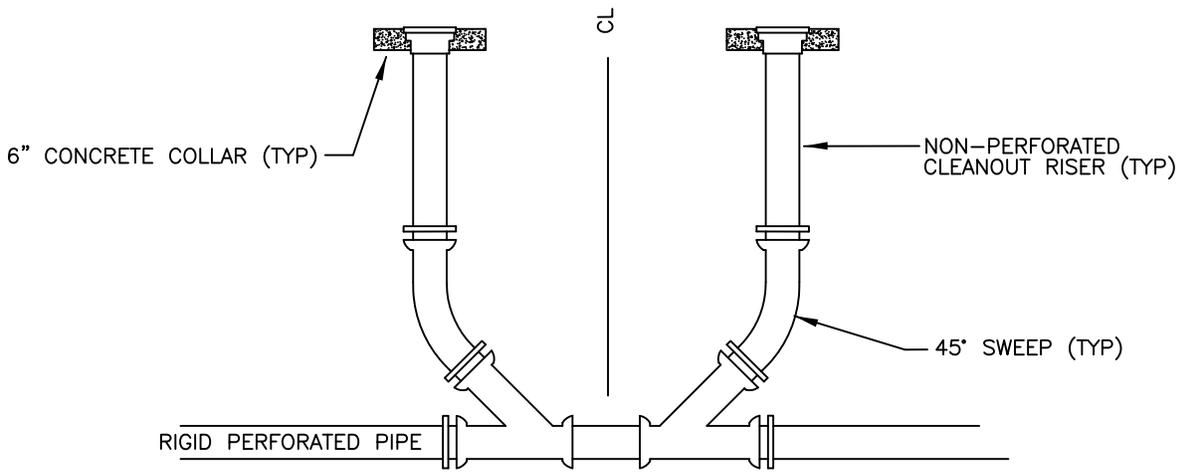
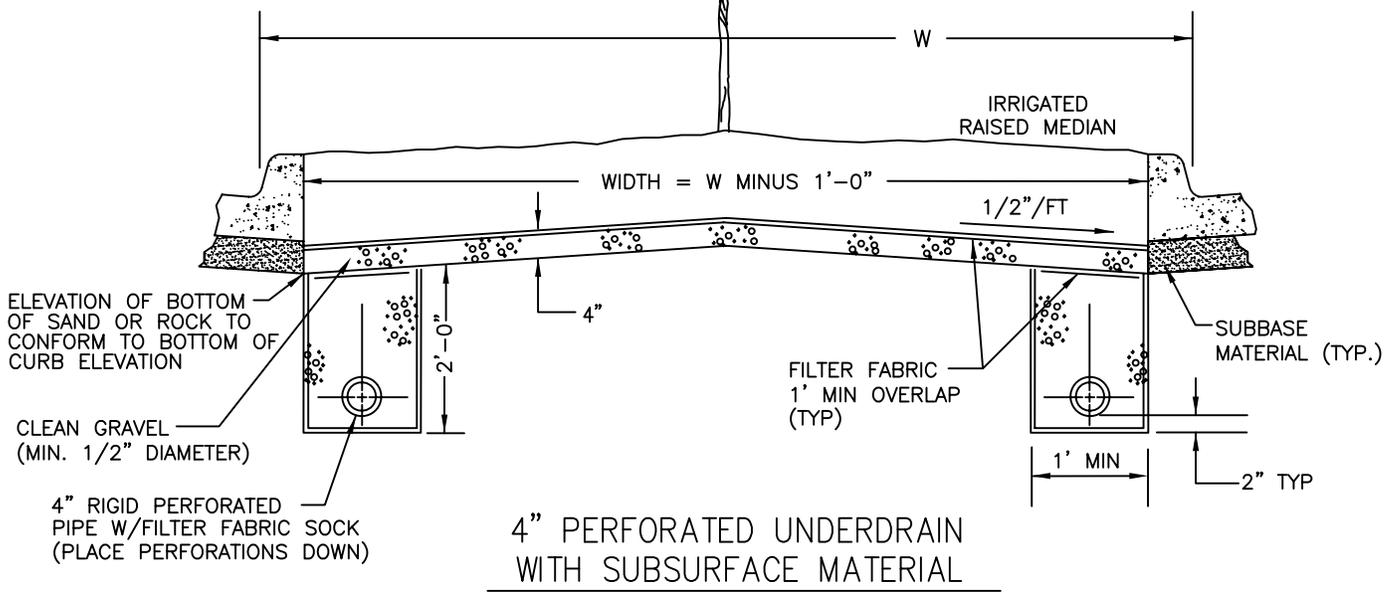
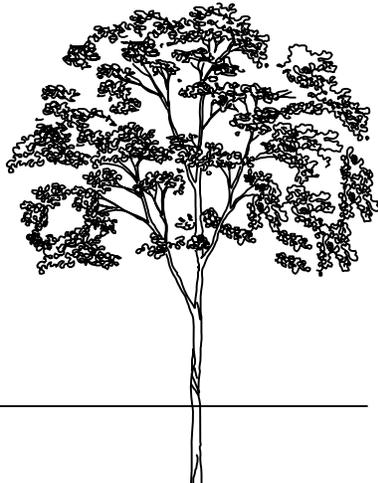
4" PERFORATED UNDERDRAIN DETAIL



NOTES

1. CURB STOP TO BE A MIN OF 1' BEHIND TRENCH.
2. PERFORATED DRAIN NEEDS TO DAYLIGHT INTO DRAINAGE SYSTEM.
3. PROVIDE PLUG ON UPSTREAM END OF PIPE.
4. PERFORATED PIPE SHALL FOLLOW ESTABLISHED GRADE AND HAVE POSITIVE FLOW.
5. THE NEED FOR UNDERDRAINS WILL BE DETERMINED BY SOILS TESTING.
6. WHERE THE BOTTOM OF SELECT MATERIAL IS GREATER THAN 4' BELOW PAVEMENT, THE UNDERDRAIN PIPE IS TO BE COINCIDENT WITH THE BOTTOM OF SELECT MATERIAL AND THE TRENCH DEPTH AND BACKFILL QUANTITY INCREASED ACCORDINGLY.
7. PLACE A VALVE BOX TOP WITH LID AND 6" THICK CONCRETE COLLAR AT ALL 4" PERFORATED UNDERDRAIN CLEANOUTS.
8. WHEN CURB & GUTTER IS IN PLACE THE CLEANOUTS SHALL BE MARKED ON THE CONCRETE CURB FACE WITH A "+".

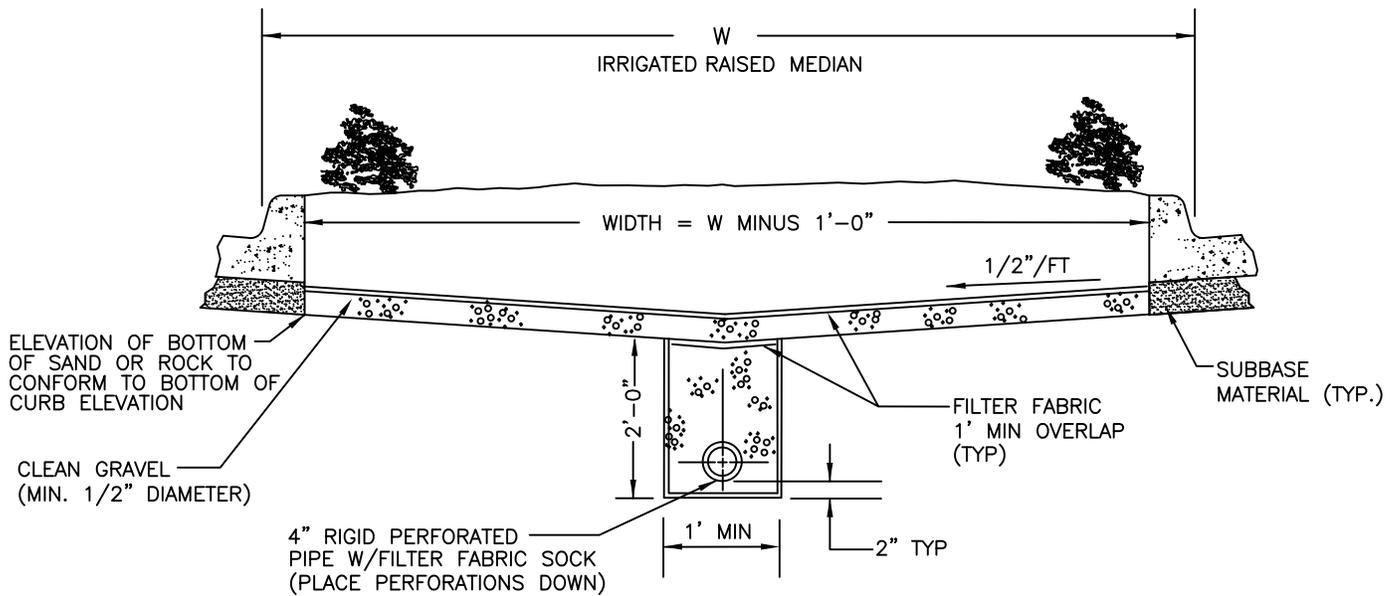




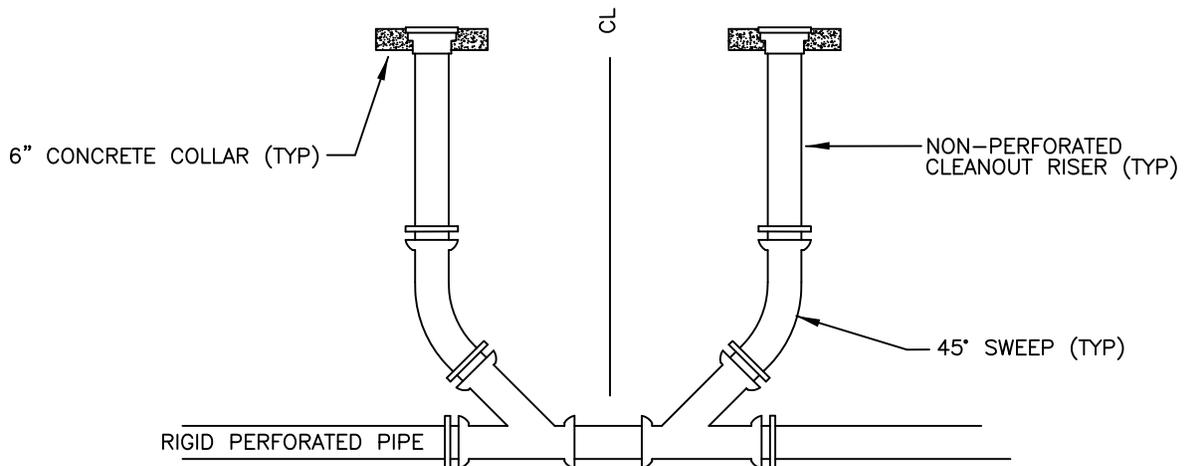
NOTES

1. PERFORATED DRAIN NEEDS TO DAYLIGHT INTO DRAINAGE SYSTEM.
2. CLEANOUTS SHALL BE SPACED A MAXIMUM OF 400'.
3. PERFORATED PIPE SHALL FOLLOW ESTABLISHED GRADE AND HAVE POSITIVE FLOW.
4. PLACE A VALVE BOX TOP WITH LID AND 6" THICK CONCRETE COLLAR AT ALL 4" PERFORATED UNDERDRAIN CLEANOUTS.
5. WHEN CURB & GUTTER IS IN PLACE THE CLEANOUTS SHALL BE MARKED ON THE CONCRETE CURB FACE WITH A "+".





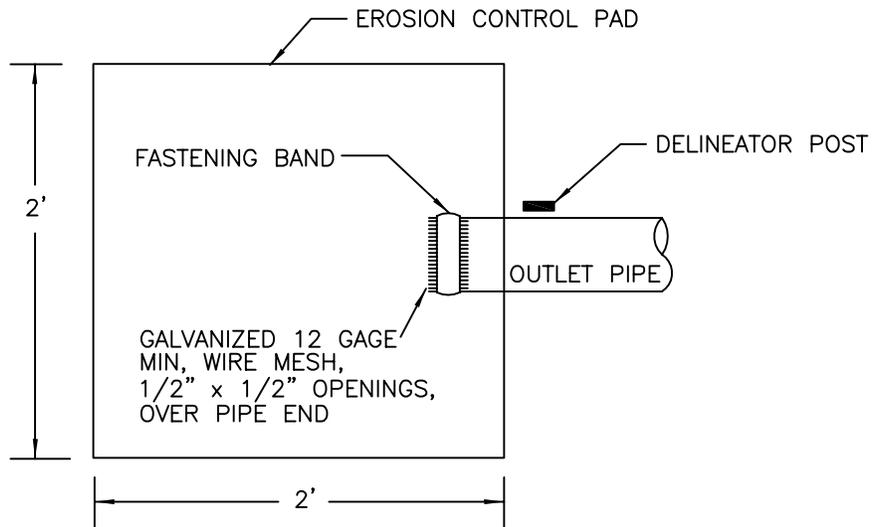
4" PERFORATED UNDERDRAIN WITH SUBSURFACE MATERIAL



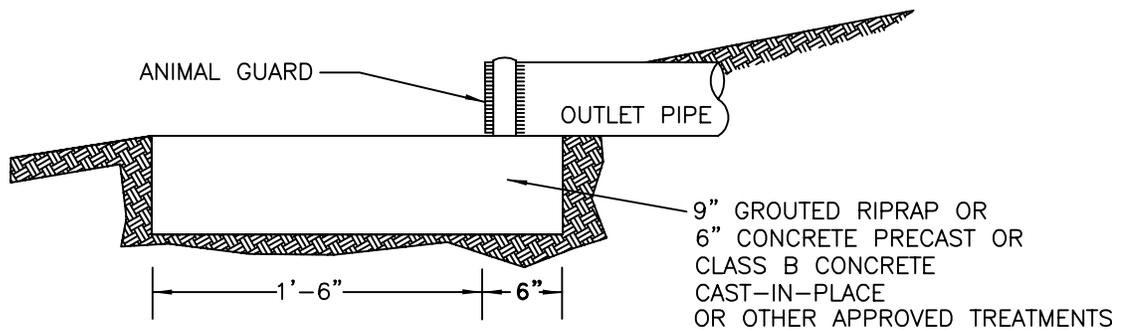
NOTES

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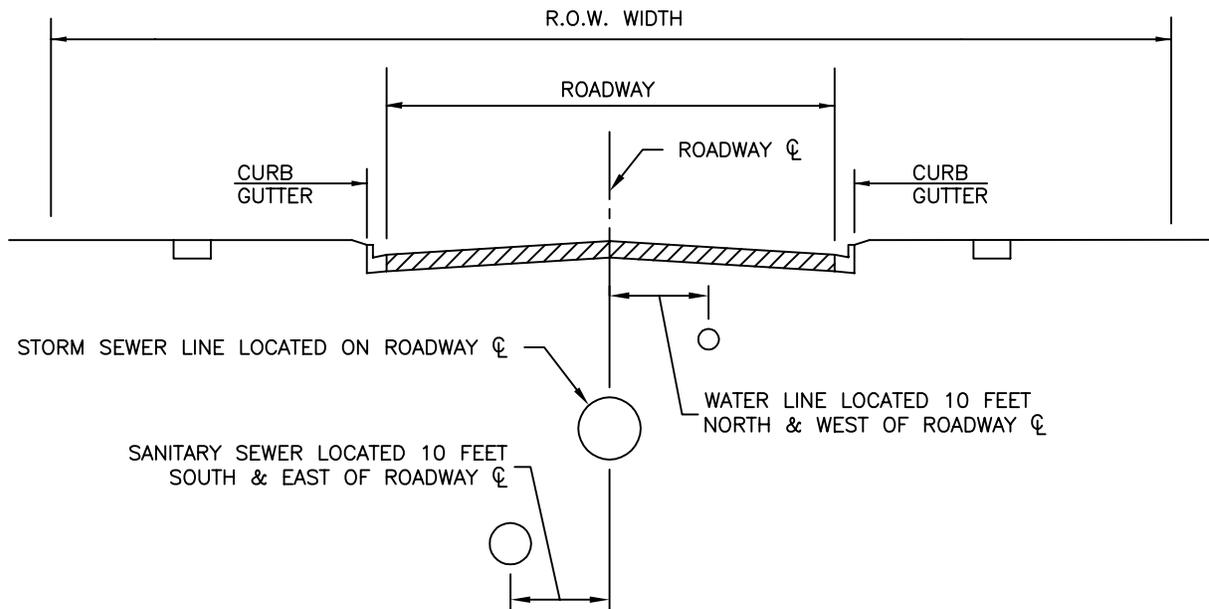
PLAN



PROFILE

OUTLET PIPE
END TREATMENT





TYPICAL UTILITY LOCATIONS ON NORMAL ROADWAY

NOTE:

1. VERTICAL DEPTH LOCATION OF UTILITIES VARIES (REFER TO LATEST TOWN OF ERIE STANDARDS AND SPECIFICATION FOR ALLOWABLE DEPTHS).
2. HORIZONTAL DIMENSIONS BASED ON CL TO CL OF VARIOUS PIPE UTILITY
3. UTILITY SEPERATION ALONG CL ROADWAY RADIAL ARC REVIEWED ON AN INDIVIDUAL PROJECT BASIS (MUST MAINTAIN 10 FEET HORIZONTAL SEPERATION)

